

#3032848-EG

2310 FAIRVIEW AVE E **STREAMLINED DESIGN REVIEW**

47 SHW
DEVELOPMENT

ADDRESS

2310 FAIRVIEW AVE E
SDCI# 3032848-EG

PROJECT TEAM

OWNER	2300 FAIRVIEW AVE E, LLC
ARCHITECT	SHW, LLC
STRUCTURAL	FRANK CO.
LANDSCAPE	ROOT OF DESIGN
SURVEYOR	CHADWICK & WINTERS

PROJECT INFO

ZONING	LR2 RC (M)
LOT SIZE	7,013 sf
FAR	1.4
ALLOWABLE FAR	9,818
PROPOSED FAR	9,620
PROPOSED UNITS	7
PARKING STALLS	7
BICYCLE PARKING	8

PROPOSAL

This project involves the demolition of two existing multi-family buildings and the construction of 7 new townhomes in two buildings with 7 parking stalls.

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PROJECT INFO / PROPOSAL

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ZONING STANDARDS

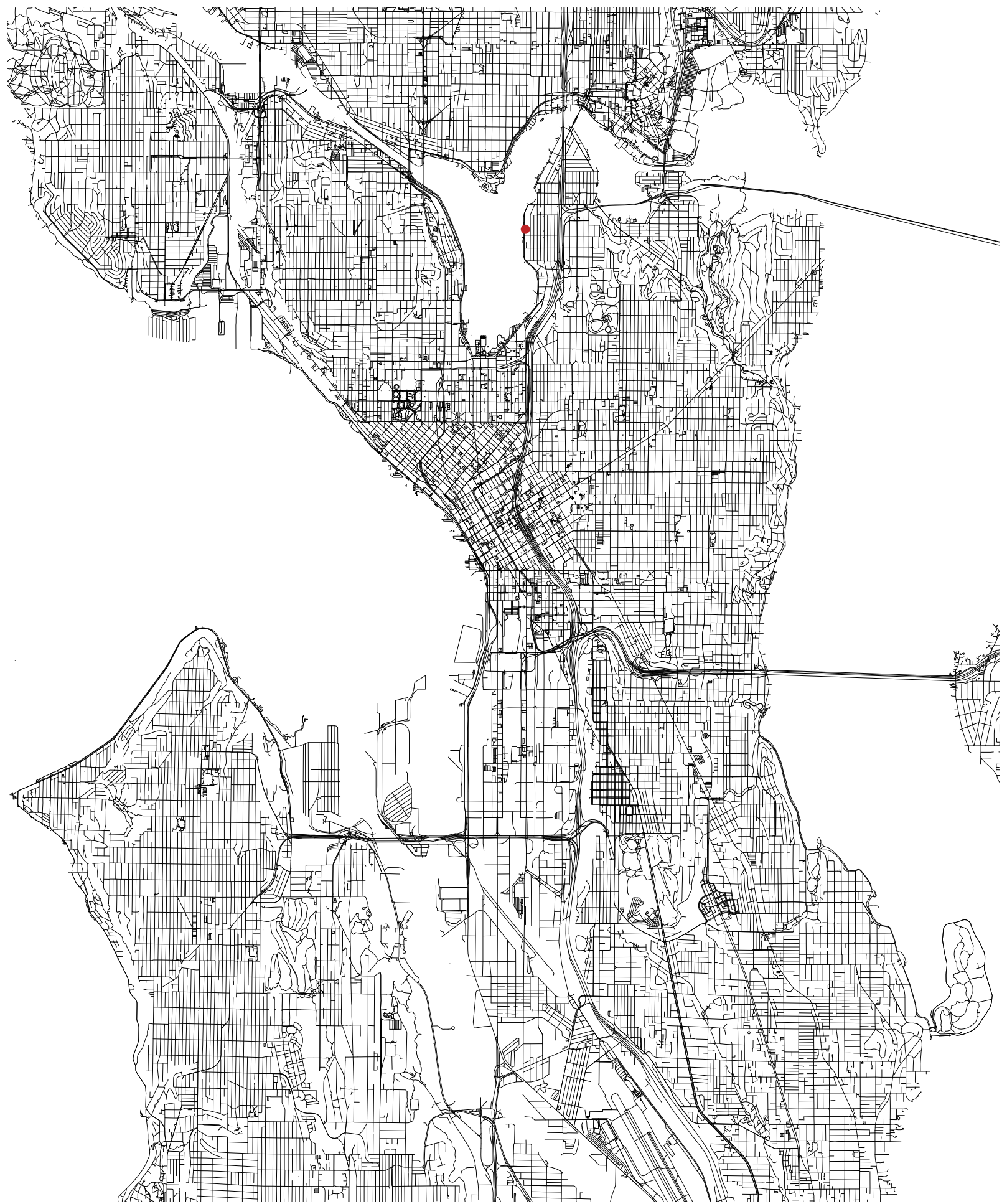
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PRIORITY GUIDELINES

22

RECENT WORK

23





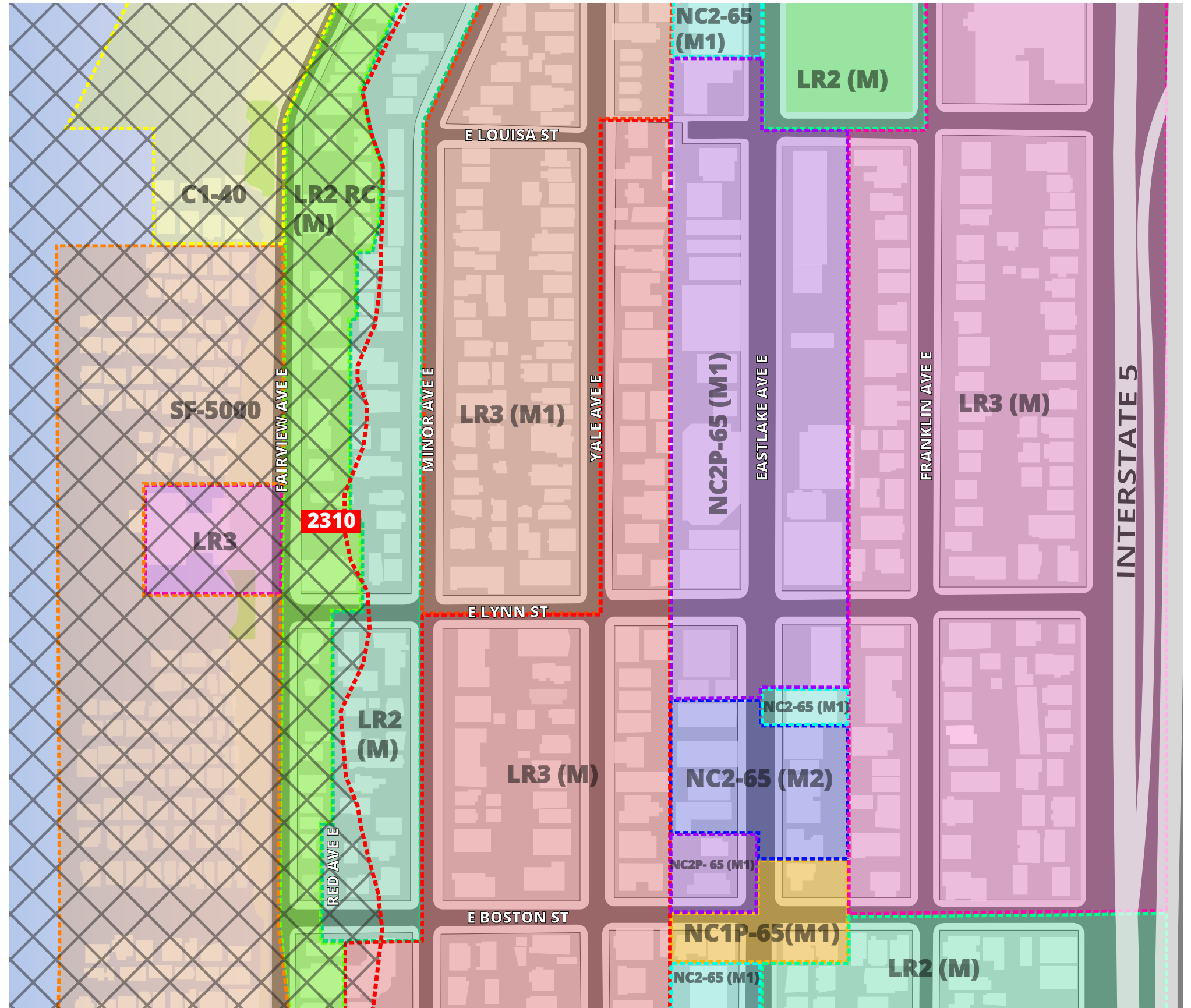
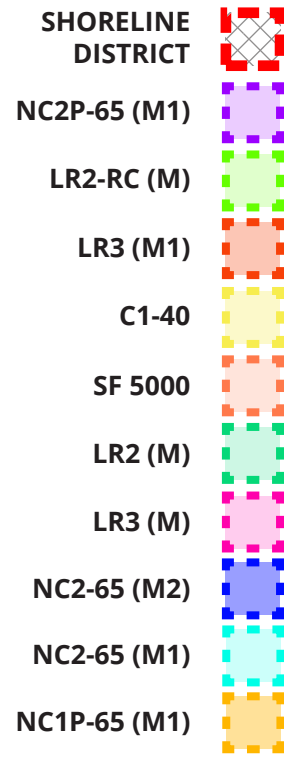
EASTLAKE

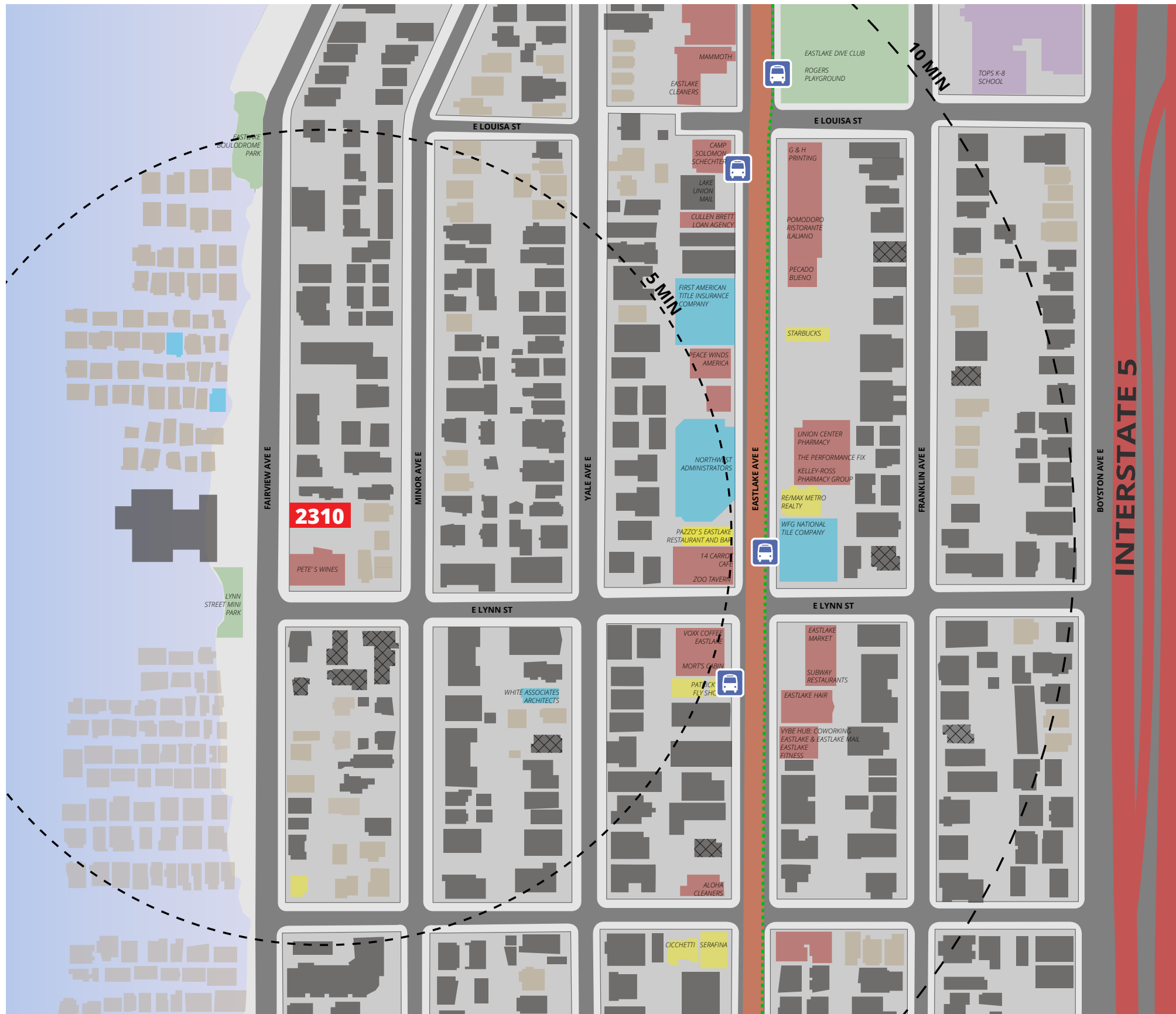
Eastlake is home to a variety of building types and uses ranging from the vibrant commercial establishments along Eastlake Ave E to the marine uses along the shore of Lake Union including floating homes and moorages. Eastlake is home to numerous small parks which contribute to the overall walkability of the neighborhood. Fairview Ave E in particular hosts several parks which invite the public to enjoy the water's edge.

ZONING

The project parcel is zoned LR2 RC (M). LR2 RC (M) zoning continues directly to the south and north for several blocks, and transitions to LR2 (M) immediately to the east. Further to the east, the neighborhood transitions to LR3 (M1). Along Eastlake Ave E the zoning shifts to NC2P-65 (M1). The floating homes directly to the west are zoned SF 5000. The subject parcel is within the Eastlake Residential Urban Village and is mapped as Frequent Transit. A portion of the site falls within a Shoreline District (UR - Urban Residential).

ZONING




















The surrounding area contains a mix of multi-family apartment buildings, single-family residences, and small commercial buildings. There is a lively commercial area two blocks east along Eastlake Ave E.

Eastlake is serviced by bus route 70 providing access to Downtown, South Lake Union, and the U-district. The area is pedestrian friendly with numerous restaurants, bars, shops and offices within walking distance of the site. There are also several mini parks in the immediate area, as well as other recreational spaces such as the I-5 Colonnade and Roger's Playground.

TPOLOGY

-  PERMITTING/PERMITTED / UNDER CONSTRUCTION
-  COMMERCIAL
-  MIXED-USE
-  MULTI-FAMILY
-  INSTITUTIONAL
-  OFFICE
-  SINGLE FAMILY
-  PUBLIC PARK

CIRCULATION

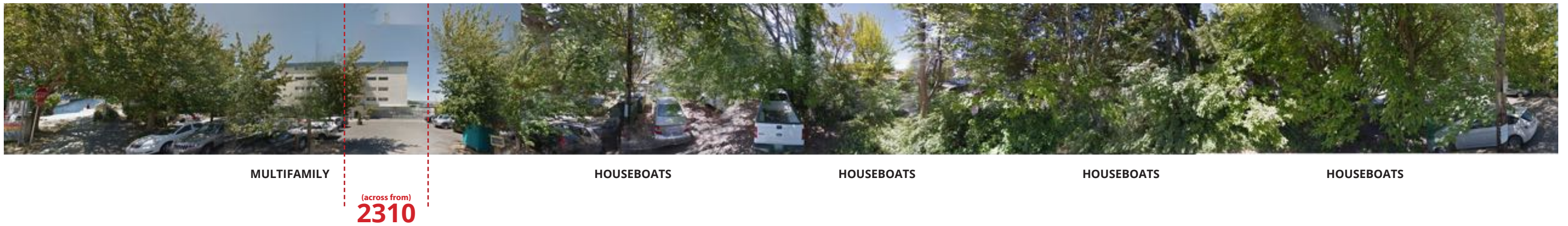
-  WALKSHED
-  MAJOR ARTERIAL
-  BIKE LANE
-  BUS STOP
-  Walk Score **78**
-  Transit Score **56**
-  Bike Score **67**



1 FAIRVIEW AVE E LOOKING EAST



2 FAIRVIEW AVE E LOOKING WEST



FAIRVIEW AVE E

The east street elevation of Fairview Ave E features low-rise multi-family apartments and townhomes with one mixed-use building immediately south of the proposed development. Parking is a mix of street parking, on-site open parking, and garage parking. The west street elevation features open parking for the entrances to floating home communities and access to public parks along the water front.



WALKING WEST ON E LYNN ST



HOUSEBOATS ALONG LAKE UNION

3 E LYNN ST LOOKING NORTH



4 MINOR AVE E LOOKING WEST



ROANOKE MINI PARK



LYNN STREET MINI PARK



GOOD TURN PARK

EASTLAKE

The portion of Fairview Ave E surrounding the project contributes to Eastlake's overall character of inviting public spaces and walkable streets. The shores along Fairview Ave E are punctuated with a number of miniparks such as Lynn Street Mini Park, Roanoke Street Mini Park, and Good Turn Park.



EXISTING CONDITIONS: SITE

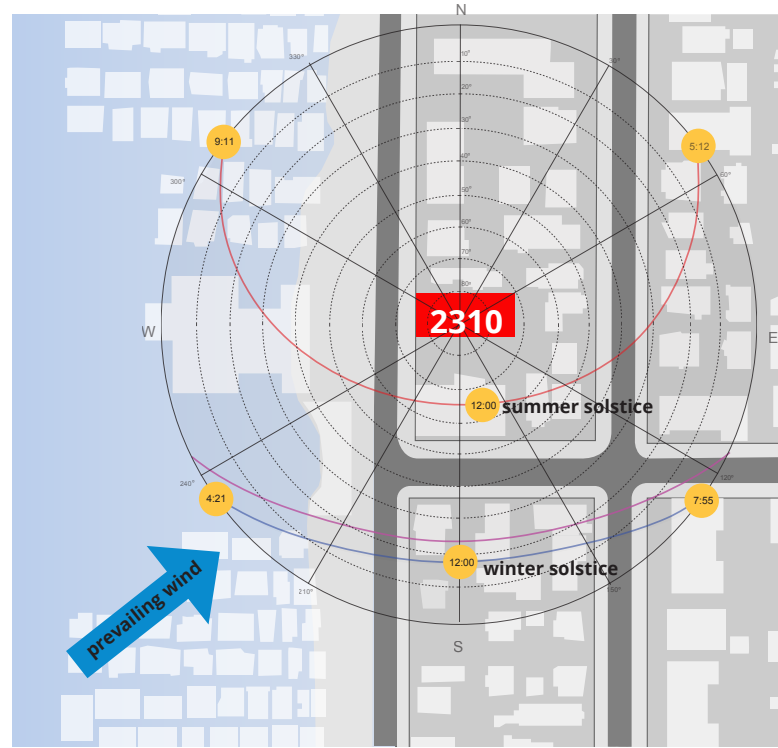
EXISTING CONDITIONS

2310 Fairview Ave E is currently occupied by two single family buildings, a shed and an above grade deck. The lot is mostly paved with parking for 7 vehicles. The western portion of the lot gently slopes up from the street with retained planting at the north property line. At approximately 100' feet from the front lot line, the terrain abruptly rises 6'-7' and then returns to a gentle slope up. Grade at the east end of the lot is retained by concrete walls along the south property line. The south property line abuts an open parking lot used for a 3-story mixed-use building further to the south. To the north, a multi-family building sits approximately 5' beyond the property line. Pavement on the lot blends into the R.O.W. with no clear delineation. There is no sidewalk, planting strip or curb in front of the lot.

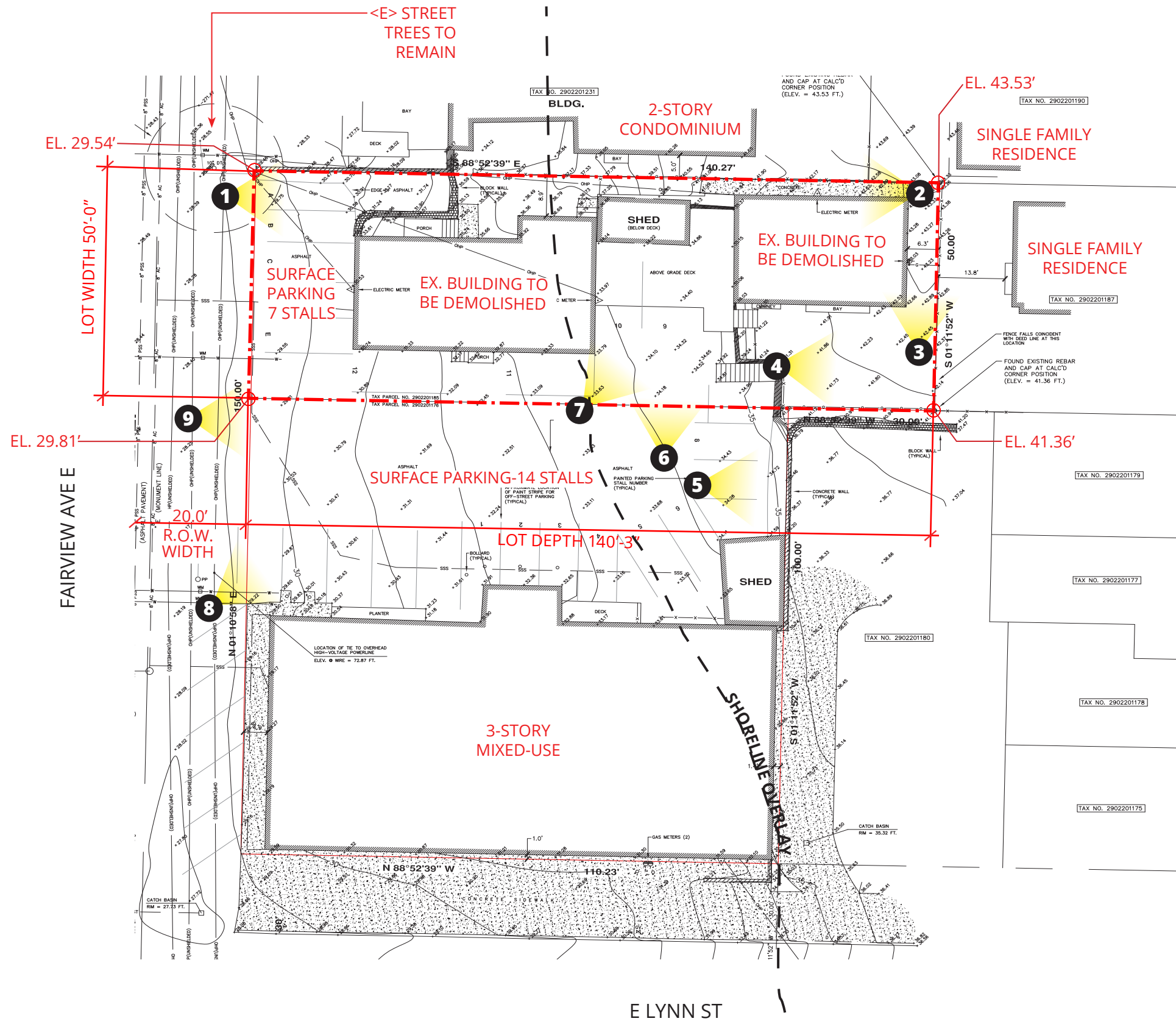
LEGAL DESCRIPTION

GREENES ADD N 20 FT OF W 60 FT OF LOT 2 & S 30 FT OF W 60 FT OF LOT 3 BLK 15 & POR OF N 20 FT OF LOT 2 OF S 30 FT OF LOT 3 BLK 16 E OF FAIRVIEW AVE & VAC ST ADJ

- **PROPERTY LINE**
- **SHORELINE OVERLAY**



SOLAR + WIND DIAGRAM

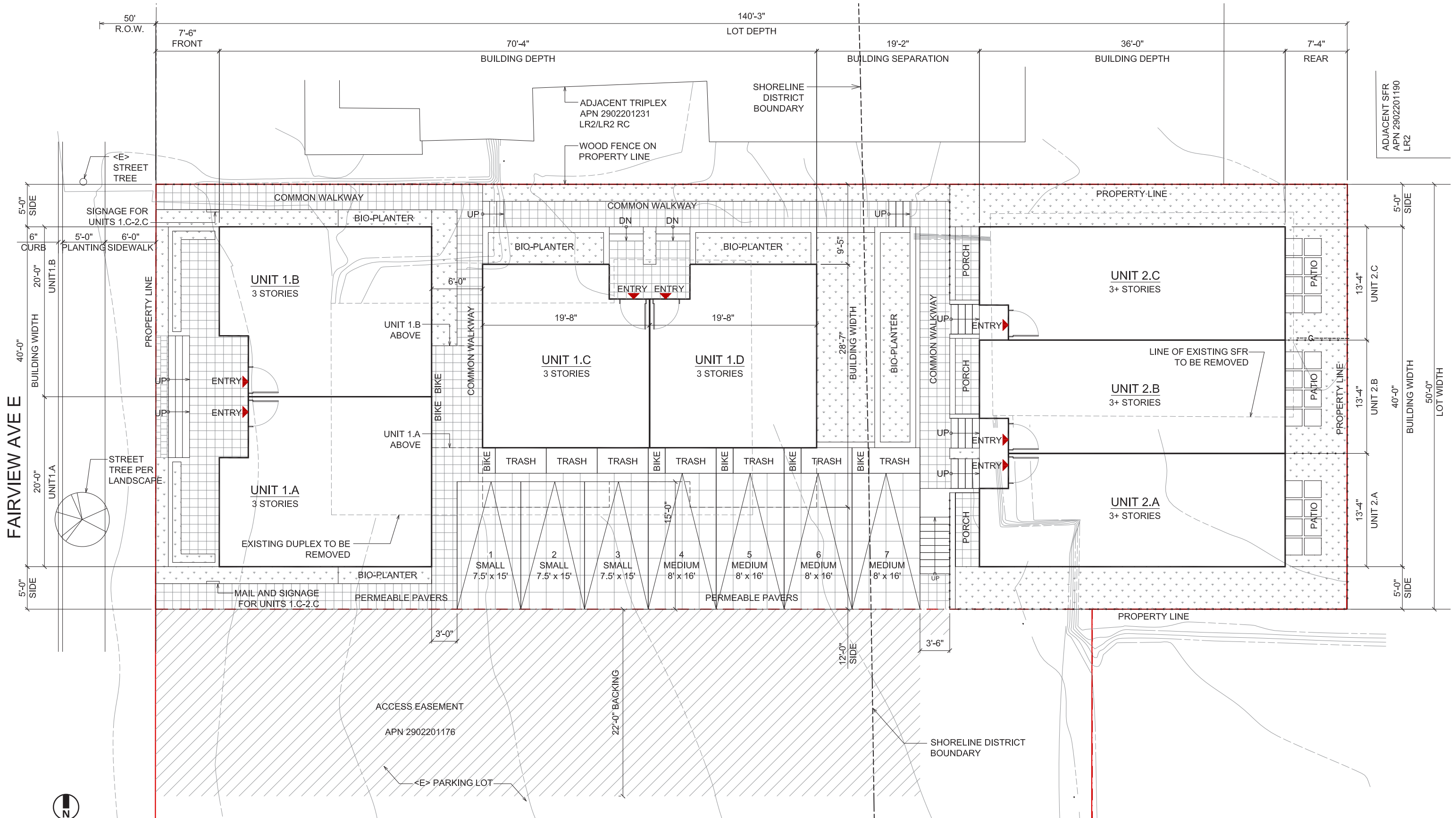


EXISTING CONDITIONS: SITE



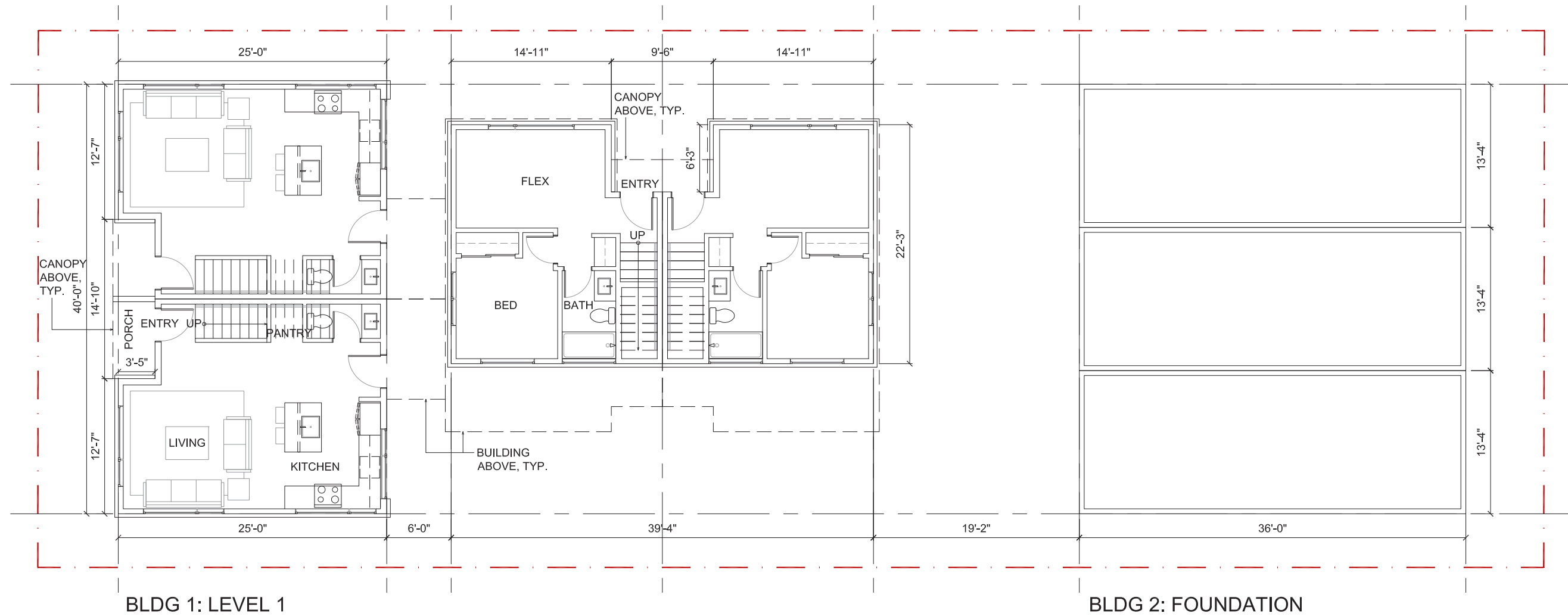
SITE PLAN

SCALE: 3/32" = 1'-0"



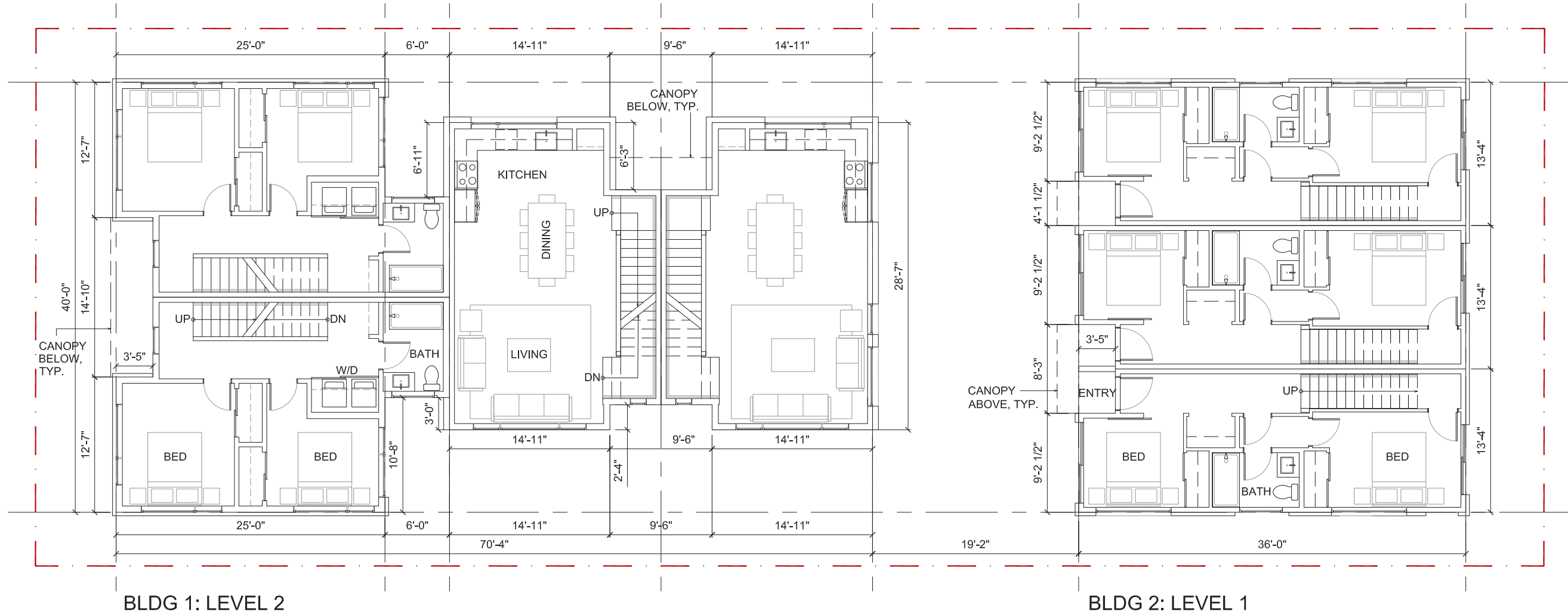
LEVEL 1 FLOOR PLAN

SCALE: 3/32" = 1'-0"



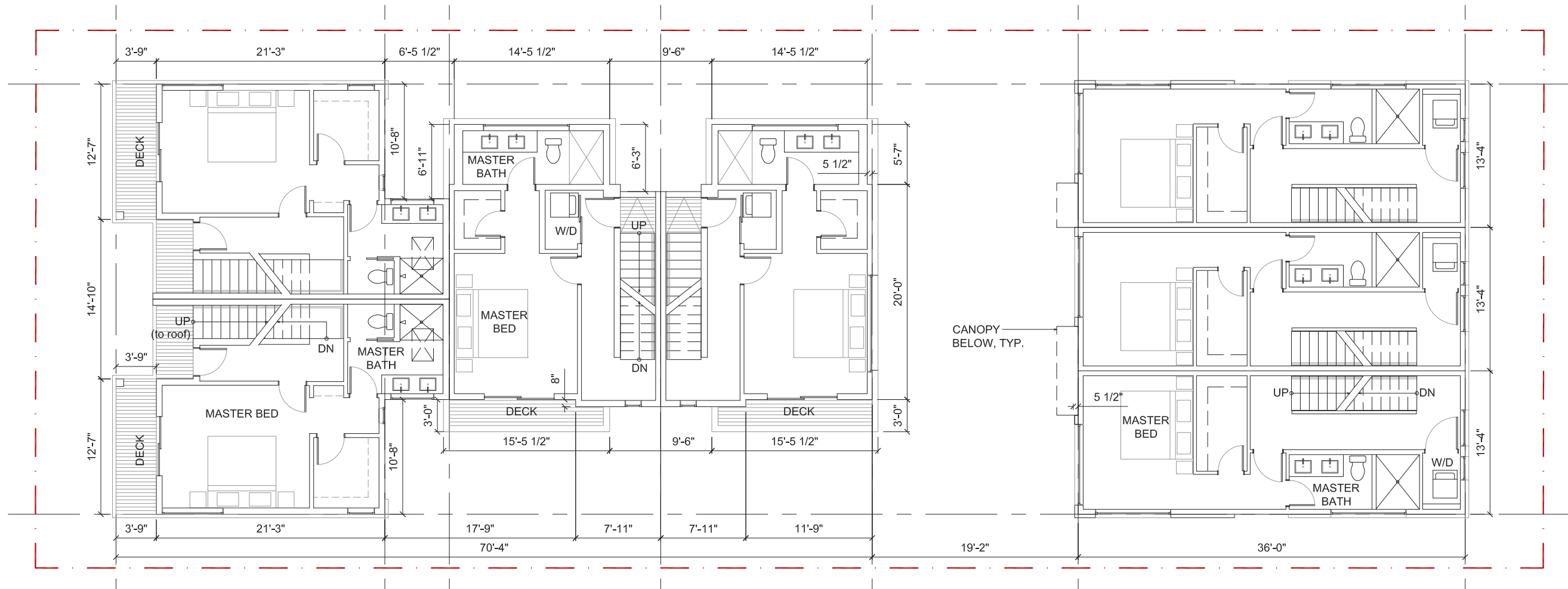
LEVEL 2 FLOOR PLAN

SCALE: 3/32" = 1'-0"



LEVEL 3 FLOOR PLAN

SCALE: 3/32" = 1'-0"



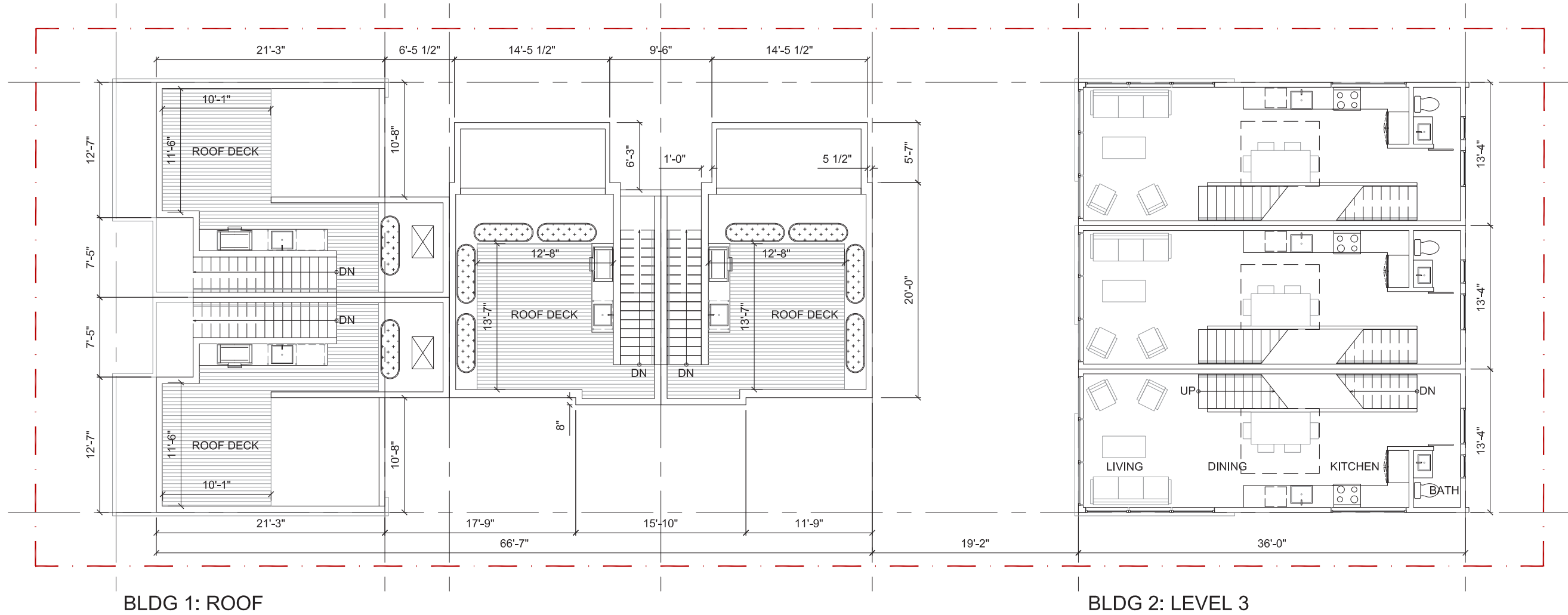
BLDG 1: LEVEL 3

BLDG 2: LEVEL 2



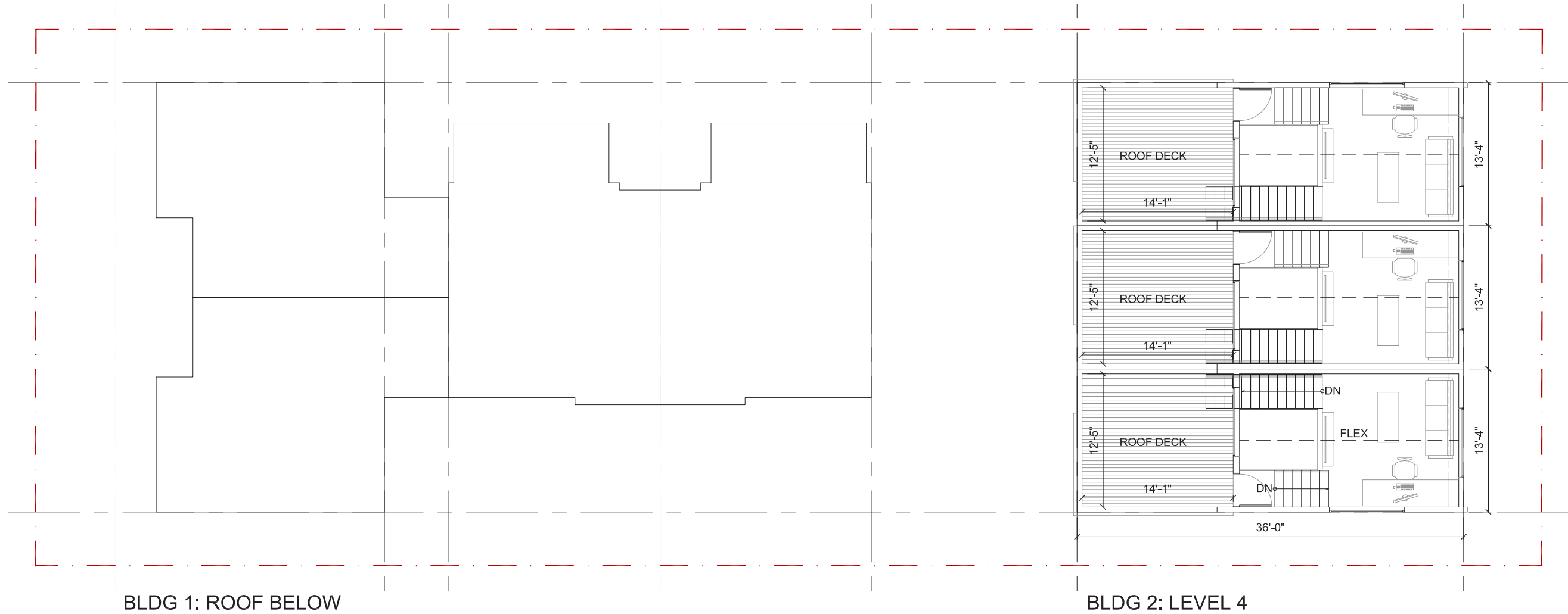
ROOF PLAN

SCALE: 3/32" = 1'-0"



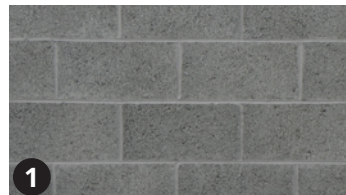
ROOF PLAN

SCALE: 3/32" = 1'-0"



WEST ELEVATION

SCALE: 3/32" = 1'-0"



1 CMU BLOCK



2 VERTICAL PLANK, CHARRED WOOD



3 FIBER CEMENT PANEL, WHITE



4 FIBER CEMENT PANEL, LIGHT GRAY



5 BLACK VINYL WINDOWS



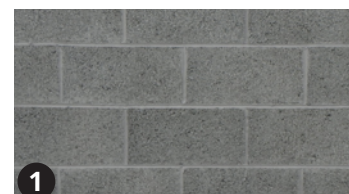
6 METAL CANOPY, BLACK



7 CONCRETE, SMOOTH FINISH

NORTH ELEVATION

SCALE: 3/32" = 1'-0"



1 CMU BLOCK



2 VERTICAL PLANK, CHARRED WOOD



3 FIBER CEMENT PANEL, WHITE



4 FIBER CEMENT PANEL, LIGHT GRAY



5 STANDING SEAM METAL SIDING



6 BLACK VINYL WINDOWS



7 CONCRETE, SMOOTH FINISH

EAST ELEVATION

SCALE: 3/32" = 1'-0"



1 CMU BLOCK



2 STANDING SEAM METAL SIDING



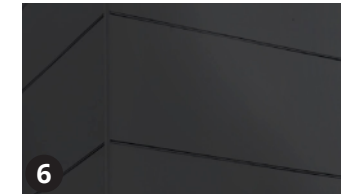
3 BLACK VINYL WINDOWS



4 CONCRETE PERMEABLE PAVERS



5 METAL CAP FLASHING, BLACK



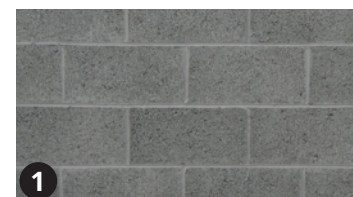
6 FIBER CEMENT PANEL, BLACK



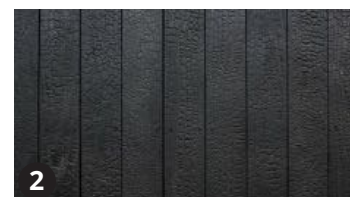
7 FIBER CEMENT PANEL, WHITE

SOUTH ELEVATION

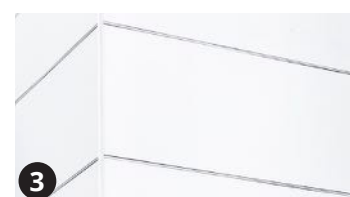
SCALE: 3/32" = 1'-0"



1 CMU BLOCK



2 VERTICAL PLANK,
CHARRED WOOD



3 FIBER CEMENT PANEL,
WHITE



4 FIBER CEMENT PANEL,
LIGHT GRAY



5 STANDING SEAM METAL
SIDING



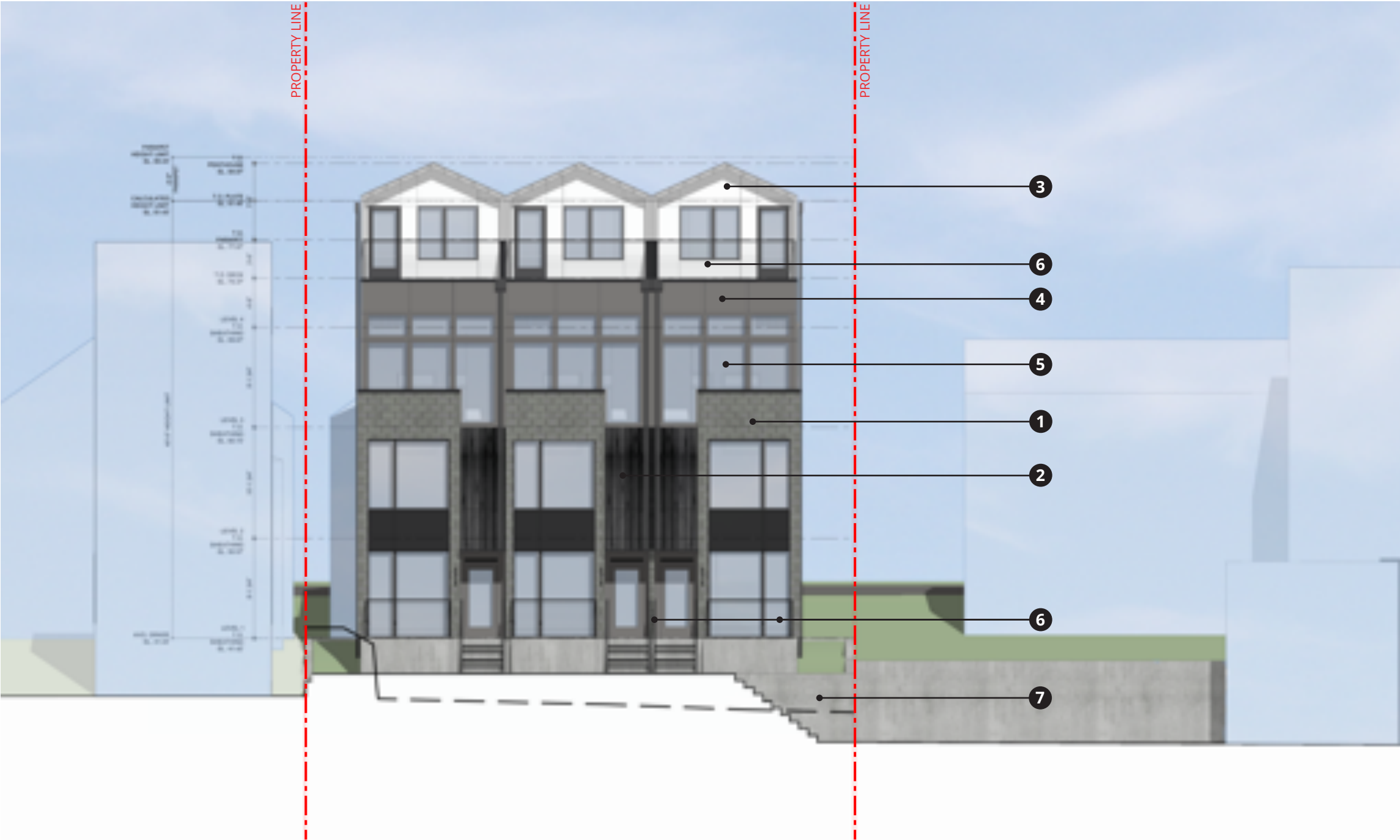
6 BIKE/TRASH ENCLOSURE,
CEDAR



7 PLANTER, CORTEN STEEL

INTERIOR WEST ELEVATION

SCALE: 3/32" = 1'-0"



1 CMU BLOCK



2 VERTICAL PLANK, CHARRED WOOD



3 FIBER CEMENT PANEL, WHITE



4 FIBER CEMENT PANEL, LIGHT GRAY



5 BLACK VINYL WINDOWS



6 METAL GUARDRAIL



7 CONCRETE, SMOOTH FINISH

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VIEW FROM FAIRVIEW AVE E

ZONING STANDARDS

ZONING: LR2 RC (M)

SMC	REQUIREMENT	PROPOSED
23.45.504	Permitted and Prohibited Uses	Residential use permitted outright.
23.45.510	Floor Area Ratio (FAR) Limits Per Table A 23.45.510, FAR for townhouses in an LR2 zone is 1.0 or 1.4 for zones with an MHA suffix.	MHA suffix allows FAR of 1.4. Lot Area: 7,013.2 SF Max Floor Area Allowed: 9,818 SF (7,013.2 X 1.4) Proposed: 9,620 SF
23.45.510.C	C.1. The structure will meet green building performance standards by earning LEED Silver rating or a Built Green 4-Star rating.	Proposed: Built Green 4-Star
23.45.512	Density Limits No density limit if project is within a zone with an MHA suffix.	Compliant: 7 dwelling units proposed, MHA suffix allows for no density limit.
23.45.514	Structure Height Maximum 30' height limit within Shoreline Zone, with exceptions for sloped roofs, overhangs, parapets & penthouses. Maximum 40' height limit for portions of the site outside the Shoreline Zone, with exceptions for sloped roofs, overhangs, parapets & penthouses.	Compliant: See elevation and section drawings with height diagram. Height calculated by averaging midpoints of facades
23.45.518	Setbacks and Separations Front: 7' Average, 5' Minimum, and 12' Minimum above 34'; Rear: 7' Average, 5' Minimum; Side: 5' Minimum; Separations Between Structures: 10' Minimum.	Front Setback: 7'-6" Min. -Compliant. Rear Setback: 7' - Compliant. Side Setbacks: 5' Front Setback: 12' Min above 34' in height on street lot line - Compliant Separation Between Buildings: 19'-2" - Compliant
23.45.522	Amenity Area 25% of lot area req'd, 50% at ground level, minimum. 7,013 SF x 25% = 1,753 SF required. 1,753 x 50% = 877 SF required at ground level.	Proposed: 2259 SF 1012 SF at grade 1247 SF at roof deck
23.45.524	Landscaping Minimum 0.6 Green Factor required, street trees required.	Compliant: Green Factor greater than 0.6 proposed, new street trees proposed.
23.45.527	Structure Width and Facade Length Limits Maximum Width 90', Maximum Facade Length: 65% of lot line, 140.25' x 65% = 91'-2" maximum facade length.	Total Maximum Structure Width: 40'-0", Compliant. Facade Length: 91'-0", Compliant.
23.54.015	Required Parking 0 parking spaces required. Parking Flexibility Area 7 permanent bicycle spaces required. 2 temporary bicycle spaces required.	Compliant: 7 medium parking spaces provided. 7 permanent bicycle spaces provided and 2 temporary bicycle spaces provided.
23.54.040	Solid Waste Storage and Access One 2'x6' storage area per dwelling unit required.	Compliant: (7) 2.5'x6' storage areas proposed.

PRIORITY GUIDELINES

CS1. Natural Systems and Site Features

B. Sunlight and Natural ventilation.

Response: The proposed design benefits from and responds to the open space created by the Pete's Market parking area. Units 1.C and 1.D feature large, south facing windows on the upper levels and a substantial separation between Buildings 1+2 brings ample light to the fronts of the triplex units and the neighboring building to the north.

CS2. Urban Patterns and Form

- A. Location in the City and Neighborhood
- B. Adjacent Sites, Streets, and Open Spaces
- C. Relationship to the Block

Response: The proposed design intends to establish a precedent for development along this portion of Fairview Ave E by introducing an organized ROW layout with a designated sidewalk and buildings which engage the street while providing appropriate setbacks for daylighting and open space.

CS3. Architectural Context and Character

- A. Emphasizing Positive Neighborhood Attributes
- B. Local History and Culture

Response: The CMU cladding featured on the street facing townhomes is a response to the eclectic variety of materials and uses found in this neighborhood which range from the vibrant commercial activity of Eastlake Ave E to the marine uses along the shore of Lake Union. This high quality material is featured at street level to exhibit the material's texture and detail and add an architectural element of interest as perceived from the street.

PL1. Open Space and Connectivity (A, B, C)

PL2. Walkability (D)

PL3. Street-Level Interaction (A, C)

PL4. Active Transit (A, B)

Response: Canopies paired with recessed entries provide clearly identifiable entry ensembles and effectively distinguish units. Roof level decks are oriented towards the street to create a dialogue between private residential spaces and the public right of way. A sizable separation between buildings allows for an unobstructed line of sight back to the rear triplex though the parking lot of Pete's Market. A pathway at the north lot line provides pedestrians easy access to all units with out having to navigate through the parking lot to the south.

DC2. Architectural Concept

- A. Massing:
- B. Architectural and Facade Composition
- C. Secondary architectural features
- D. Scale and Texture

Response: Building massing is organized into strong and simple volumes and is reinforced by the composition of cladding materials. Material transitions effectively break down the overall building scale and respond to the scale of surrounding buildings.

DC3. Open Space Concept

- A. Building-Open Space Relationship
- B. Open Spaces and Activities
- C. Design

Response: The project provides a variety of open space conditions that expand and connect the interiors. Units 1.C and 1.D are oriented to the south to take advantage of the open space created by parking for Pete's Market. Floor to ceiling glazing on the street facing units opens up to porches overlooking the street.

DC4. Exterior Elements and Material

- A. Exterior Elements and Finishes
- D. Trees, Landscape and Hardscape Materials

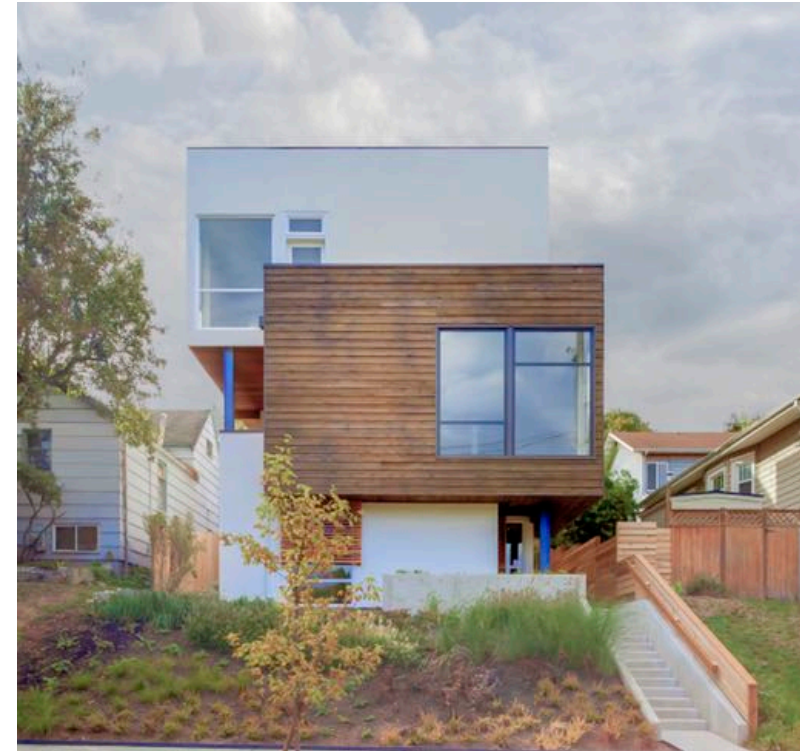
Response: Plants are chosen and arranged at the front porches facing the street to provide a point of interest for the public realm as experienced from the street and sidewalk while providing an appropriate level of privacy for the townhome inhabitants.



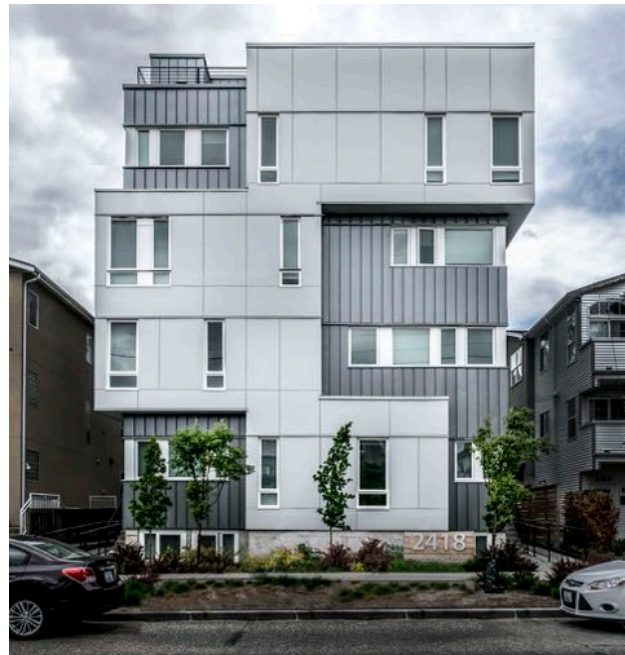
A



B



C



D



E



F

RECENT WORK
SHW

- A** 14TH AVE NW TOWNHOMES
5806 14TH AVE NW / SEATTLE, WA
- B** 5902 ROWHOUSES
5902 22ND AVE NW / SEATTLE, WA
- C** 225 HOUSE
225 27TH AVE E / SEATTLE, WA

- D** 2418 BALLARD APARTMENTS
2418 NW 58TH ST / SEATTLE, WA
- E** 16TH AVE TOWNHOMES
1114 16TH AVE / SEATTLE, WA
- F** E HAMLIN DUPLEX
111 E HAMLIN ST / SEATTLE, WA